



Staff Report to the Zoning Administrator

Application Number: **141099**

Applicant: Stacy and Witbeck Inc, Attention: Michael Dorsa **Agenda Date:** 8/1/2014

Owner: La Selva Beach Improvement Association, Santa Cruz County Regional Transportation Commission **Agenda Item #:** 2

APN: 045-161-05, 045-022-04, 045-201-16, **Time:** After 9:00 a.m.

Project Description: Proposal to replace the La Selva Beach railroad trestle and to grade approximately 150 cubic yards of material for creation of temporary access for a crane pad during the construction period on APNs 045-161-05 and 045-201-16 and to install a temporary sheet pile wall and crane pad on APN 045-022-04. Requires a Coastal Development Permit and Preliminary Grading Review.

Location: The project site is located at the intersection of Breve, Arbolado and Margarita Road through the La Selva Beach Improvement Association parking lot gate.

Supervisorial District: 2nd District (District Supervisor: Friend)

Permits Required: Requires a Coastal Development Permit, Riparian Exception

Technical Reviews: Preliminary Grading Review

Staff Recommendation:

- Determine that the proposal is exempt from further Environmental Review under the California Environmental Quality Act.
- Approval of Application 141099, based on the attached findings and conditions.

Exhibits

- | | |
|---|---|
| A. Categorical Exemption (CEQA determination) | E. Assessor's, Location, Zoning and General Plan Maps |
| B. Findings | F. Agency comments |
| C. Conditions | |
| D. Project plans | |

Parcel Information

Parcel Size:

Railroad trestle west-APN 045-022-

04: 2.8 acres

Railroad trestle east- APN 045-201-

16: 2.5 acres

La Selva Beach Association-APN

045-161-05: 1.5

Existing Land Use - Parcel:

Railroad trestle

Existing Land Use - Surrounding:

Beach

Project Access:

Intersection of Breve, Arbolado and Margarita Road through the La Selva Beach Improvement Association Parking lot

Planning Area:

La Selva

Land Use Designation:

See Attached Map (Exhibit E)

Railroad trestle west- 045-022-04: O-U (Urban Open Space) O-R (Existing Parks and Recreation)

Railroad trestle east -045-201-16: O-U (Urban Open Space)

045-161-05:

La Selva Beach Improvement Association- 045-161-05: O-U (Urban Open Space)

Zone District:

See Attached Map (Exhibit E)

Railroad trestle west- 045-022-04: PF (Public Facility)

Railroad trestle east - 045-201-16: PR (Existing Parks and Recreation)

La Selva Beach Improvement Association- 045-161-05: PR (Existing Parks and Recreation)

Coastal Zone:

Inside Outside

Appealable to Calif. Coastal Comm.

Yes No

Environmental Information

Geologic Hazards:

Trestle above ephemeral stream area located within Flood Zone A, in-kind repair only with no additional displacement of flood water anticipated

Soils:

N/A, trestle improvements to comply with Federal Railroad Administration standards

Fire Hazard:

Not a mapped constraint

Slopes:

0-50 percent

Env. Sen. Habitat:

Mapped intermittent stream, but classified as an ephemeral stream; the extent of riparian areas related to ephemeral drainages is the area between the bank full flow lines, which is fully confined between the existing retaining walls. Thus, this project does not require a riparian exception as the proposed work will be located on the slopes above the walls that confine the drainage through this area. No work is

- proposed within the streambed.
- Grading:** Grading is required for temporary pad construction and pier construction; the project is conditioned to obtain a grading permit prior to construction to address final grading, drainage, and erosion control.
- Tree Removal:** No trees are proposed to be removed; however removal of vegetation is required for the temporary pads. Vegetation removal is not proposed within an identified sensitive habitat. A re-vegetation plan is required to be reviewed and approved prior to construction and a monitoring required ensuring plant survival.
- Scenic:** Mapped as scenic, however structural rehabilitation of the trestle will not result in significant modification to the existing visual environment.
- Drainage:** No changes to existing drainage proposed. Plans include best management practices to ensure that water quality is protected during construction.
- Archeology:** Not mapped/no physical evidence on site

Services Information

- Urban/Rural Services Line: x Inside ___ Outside
Water Supply: N/A
Sewage Disposal: N/A
Fire District: La Selva Fire Protection District
Drainage District: Zone 4

Background

On October 12, 2012 the Regional Transportation Commission purchased the Santa Cruz Branch Rail Line (Branch Line) from Union Pacific with funds made available by the California Transportation Commission. The railroad corridor extends from Davenport to Watsonville and runs parallel to the Highway 1 corridor. Currently the Branch Rail Line is a railroad under federal jurisdiction by the Federal Railroad Administration with Iowa Pacific Holdings operating a freight business known as the Santa Cruz & Monterey Bay Railway. Replacement of the La Selva trestle was identified by the Regional Transportation Commission as rehabilitation necessary to maintain the structural integrity of the freight line. As directed by the Federal Railroad Administration, construction design is subject to the American Railway Engineering and Maintenance of Railway Association standards and project construction inspections are conducted by contract inspectors. Construction is not regulated by the County of Santa Cruz.

Project Setting/Project Description

The existing trestle is located on the edge of the coastal bluff in an area commonly known as the La Selva beach community and crosses an unnamed ephemeral stream. The trestle is comprised of rail plates and girders supported by structural supports (bents) and anchored to piers located at intervals across the stream channel.

The project involves the removal and replacement of structural steel associated with the trestle.

The piers are proposed to be partially demolished and replaced. No actual work is proposed within the stream channel or within the beach area. See attached project plans (Exhibit D).

Construction requires temporary crane pads (Exhibit D) proposed on each side of the trestle, a staging area located in the La Selva Beach Improvement Association parking lot, and man lift-mini-excavator work areas located between bent 3 and 4 on the east side and located between bent 6 and 8 on the west side. Excavator pads (Exhibit D) are approximately 600 to 650 square feet in size and are required for demolition and construction of structural pier foundations and trestle assembly.

Proposed grading is associated with the temporary access, pad development and pier foundation development. Excavations required for pad development and piers are proposed to be backfilled and restored to the original ground contours from existing materials excavated during construction. Plans include proposed erosion control fiber rolls located below each bent and temporary silt fencing between the last bent and the intermittent stream. Plans include a re-vegetation plan that will be reviewed and approved prior to construction, as required by Environmental Planning.

The new trestle is proposed to be enviro-green color as selected by the La Selva Beach Improvement Association.

Zoning & General Plan Consistency

The subject property is located in the PR (Existing Parks and Recreation) zone district and PF (Public Facility) zone district, designations which allow railroad uses. The proposed train trestle improvements are a permitted use within the zone district and the zoning is consistent with the site's (OR, O-U) Existing Parks and Recreation, Urban Open Space General Plan designation.

Local Coastal Program Consistency

The proposed train trestle improvements are in conformance with the County's certified Local Coastal Program in that the trestle improvements do not significantly alter the existing trestle or result in significant modification to the visual appearance. The proposed green color is visually compatible with the character of the surrounding natural vegetative environment, comprised of Monterey Cypress trees.

The project site is located between the shoreline and the first public road but is not identified as a priority acquisition site in the County's Local Coastal Program. However, three coastal access trails exist, two on the northwest side and one on the southeast side of the trestle. The proposed project will require closure of the access points on the northwest side during construction and partial closure of the trail on the southeast side. Project construction is expected to take approximately four months. The applicant has indicated that an agreement with the La Selva Beach Improvement Association requires that beach access located on the southeast side will be maintained for the duration of the four months of construction with exception of a few weeks. Trail closure is temporary and will not preclude coastal access once construction is completed, which is consistent with required coastal access protection.

Environmental Review

A preliminary determination has been made that the project is exempt from the California Environmental Quality Act under Class 1, Section 15301 and allows maintenance activities to existing facilities including railways to comply with public safety requirements. A notice of exemption has been attached as Exhibit A.

Conclusion

As proposed and conditioned, the project is consistent with all applicable codes and policies of the Zoning Ordinance and General Plan/LCP. Please see Exhibit "B" ("Findings") for a complete listing of findings and evidence related to the above discussion.

Staff Recommendation

- Determine that the proposal is exempt from further Environmental Review under the California Environmental Quality Act.
- **APPROVAL** of Application Number 141099, based on the attached findings and conditions.

Supplementary reports and information referred to in this report are on file and available for viewing at the Santa Cruz County Planning Department, and are hereby made a part of the administrative record for the proposed project.

The County Code and General Plan, as well as hearing agendas and additional information are available online at: www.co.santa-cruz.ca.us

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Coastal Development Permit Findings

1. That the project is a use allowed in one of the basic zone districts, other than the Special Use (SU) district, listed in section 13.10.170(d) as consistent with the General Plan and Local Coastal Program LUP designation.

This finding can be made, in that the property is zoned PR (Existing Parks and Recreation) and PF (Public Facility), and General Plan Policy 2.21.2 states "allow public facility uses in all urban residential land use designation and zoning districts as well as limited public facility uses in commercial designations and districts as regulated in Volume II of the County Code."

Therefore, railroad improvements are allowed uses within the "PR" and "PF" zone districts. Public Facility uses are also noted as allowed uses in the "Parks and Recreation" zone districts. Thus, proposed train trestle improvements are a principal permitted use within the zone district, and the zoning is consistent with the site's (OR, O-U) Existing Parks and Recreation, Urban Open Space General Plan designation.

2. That the project does not conflict with any existing easement or development restrictions such as public access, utility, or open space easements.

This finding can be made, in that although three existing pedestrian trails are located along the edge of the stream channel to the beach, access is only proposed to be blocked during construction (approximately four months) and will not result in permanent closure.

3. That the project is consistent with the design criteria and special use standards and conditions of this chapter pursuant to section 13.20.130 et seq.

This finding can be made, in that the proposed trestle improvements are consistent with the surrounding natural character of bluff top as the proposed enviro-green color will blend into the background of the surrounding coastal vegetation; and although the trestle is located on a bluff top in a visible location along the beach, the proposed trestle rehabilitation work does not expand or significantly alter the existing trestle.

4. That the project conforms with the public access, recreation, and visitor-serving policies, standards and maps of the General Plan and Local Coastal Program land use plan, specifically Chapter 2: figure 2.5 and Chapter 7, and, as to any development between and nearest public road and the sea or the shoreline of any body of water located within the coastal zone, such development is in conformity with the public access and public recreation policies of Chapter 3 of the Coastal Act commencing with section 30200.

This finding can be made, in that the project site is located between the shoreline and the first public road. The train trestle rehabilitation will support continued freight business historically associated with the railroad and will otherwise not permanently interfere with existing public access to the beach, ocean, or any nearby body of water as trail access to the beach is provided below the trestle. Further, the project site is not identified as a priority acquisition site in the County Local Coastal Program. Temporary trail closure is required during project construction. However, the applicant has agreed with the La Selva Beach Improvement Association to maintain access on the southeast side of the trestle as much as feasible during the four month construction process.

5. That the proposed development is in conformity with the certified local coastal program.

This finding can be made, in that the trestle rehabilitation improvements are designed to be visually compatible, in scale, and integrated with the character of the surrounding area by incorporation of a natural green color selected by the La Selva Beach Improvement Association. Furthermore, General Plan Policy 2.21.2 states “allow public facility uses in all urban residential land use designation and zoning districts as well as limited public facility uses in commercial designations and districts as regulated in Volume II of the County Code.” Therefore, railroad improvements are allowed uses within the “PR” and “PF” zone districts.

Development Permit Findings

1. That the proposed location of the project and the conditions under which it would be operated or maintained will not be detrimental to the health, safety, or welfare of persons residing or working in the neighborhood or the general public, and will not result in inefficient or wasteful use of energy, and will not be materially injurious to properties or improvements in the vicinity.

This finding can be made, in that the project is located in an area designated for railroad uses and is not encumbered by physical constraints to development. Railroads are subject to Federal design standards and railway improvements are required to comply with prevailing Federal design standards to insure the optimum in safety and the conservation of energy and resources. Improvements to the train trestle will improve health and safety to surrounding property and the improvements in the vicinity and will thus not be material injurious to the surrounding area.

2. That the proposed location of the project and the conditions under which it would be operated or maintained will be consistent with all pertinent County ordinances and the purpose of the zone district in which the site is located.

This finding can be made, in that the General Plan Policy 2.21.2 states “allow public facility uses in all urban residential land use designation and zoning districts as well as limited public facility uses in commercial designations and districts as regulated in Volume II of the County Code.” Therefore, railroad improvements are allowed uses within the “PR” and “PF” zone districts. Public Facility uses are also noted as allowed uses in the “Parks and Recreation” zone districts. Thus, proposed train trestle improvements are a principal permitted use within the zone district, and the zoning is consistent with the site's (OR, O-U) Existing Parks and Recreation, Urban Open Space General Plan designation.

3. That the proposed use is consistent with all elements of the County General Plan and with any specific plan which has been adopted for the area.

This finding can be made, in that the existing railroad use and proposed trestle improvements are consistent with the use and density requirements specified for the Existing Parks and Recreation, Urban Open Space (OR, O-U) land use designation in the County General Plan.

The proposed train trestle improvements will not adversely impact the light, solar opportunities, air, and/or open space available to other structures or properties.

A specific plan has not been adopted for this portion of the County.

4. That the proposed use will not overload utilities and will not generate more than the acceptable level of traffic on the streets in the vicinity.

This finding can be made, in that the proposed train trestle improvements are to be constructed on an existing railway trestle. The proposed trestle improvements will not affect utilities or expected level of traffic.

5. That the proposed project will complement and harmonize with the existing and proposed

Application #: 141099
APN: 045-161-05
Owner: La Selva Beach Improvement Association

land uses in the vicinity and will be compatible with the physical design aspects, land use intensities, and dwelling unit densities of the neighborhood.

This finding can be made, in that the proposed trestle improvements incorporate a green color selected by the La Selva Improvement Association that is compatible with the natural vegetative background of the Monterey Cypress trees typical of the La Selva area.

6. The proposed development project is consistent with the Design Standards and Guidelines (sections 13.11.070 through 13.11.076), and any other applicable requirements of this chapter.

This finding can be made, in that the proposed train trestle improvements will not change the character or scale of the railway in that location. The proposed green color will enhance the aesthetic qualities of the surrounding area and will not visually impact available open space in the surrounding area.

Conditions of Approval

Exhibit D: Project Plans

- I. This permit authorizes replacement of the La Selva Beach railroad trestle for creation of temporary access for a crane pad during the construction period on APNs 045-161-05 and 045-201-16 and to install a temporary sheet pile wall and crane pad on APN 045-022-04. This approval does not confer legal status on any existing structure(s) or existing use(s) on the subject property that are not specifically authorized by this permit. Prior to exercising any rights granted by this permit including, without limitation, any construction or site disturbance, the applicant/owner shall:
 - A. Sign, date, and return to the Planning Department one copy of the approval to indicate acceptance and agreement with the conditions thereof.
 - B. Obtain required streambed alteration agreement by State Fish and Wildlife and/or Federal Army Corps permit, as necessary.
 - C. Comply with Federal Railroad Administration regulations and obtain any required federal permit prior to construction, as necessary.
 - D. Obtain a Grading Permit from the Santa Cruz County Planning Department.
 1. Any outstanding balance due to the Planning Department must be paid prior to making a Building Permit application. Applications for Building Permits will not be accepted or processed while there is an outstanding balance due.
 - E. Obtain right-of-entry authorizations from all site work located on private property.
- II. Prior to issuance of a Grading Permit:
 - A. Submit final architectural plans for review and approval by the Planning Department. The final plans shall be in substantial compliance with the plans marked Exhibit "D" on file with the Planning Department. Any changes from the approved Exhibit "D" for this development permit on the plans submitted for the Building Permit must be clearly called out and labeled by standard architectural methods to indicate such changes. Any changes that are not properly called out and labeled will not be authorized by any Building Permit that is issued for the proposed development. The final plans shall include the following additional information:
 1. One elevation shall indicate materials and colors as they were approved by this Discretionary Application.
 2. Final grading, drainage, and erosion control plans.

3. Submit a re-vegetation monitoring program to achieve a minimum 80% survival rate of the trees and shrubs after 5 years. If 80% survival rate is not achieved after 5 years, the monitoring program shall continue until 80% is achieved.
- III. Prior to Construction, an onsite pre-construction meeting shall be held with County Environmental Planning staff, the project civil engineer, the geotechnical engineer, the contractor and the re-vegetation specialist.
 - IV. All construction shall be performed according to the approved plans for the Grading Permit. Prior to final Grading Permit inspection, the applicant/owner must meet the following conditions:
 - A. All site improvements shown on the final grading plans shall be installed.
 - B. Pursuant to Sections 16.40.040 and 16.42.080 of the County Code, if at any time during site preparation, excavation, or other ground disturbance associated with this development, any artifact or other evidence of an historic archaeological resource or a Native American cultural site is discovered, the responsible persons shall immediately cease and desist from all further site excavation and notify the Sheriff-Coroner if the discovery contains human remains, or the Planning Director if the discovery contains no human remains. The procedures established in Sections 16.40.040 and 16.42.080, shall be observed.
 - C. A geotechnical engineer must inspect and analyze the temporary cut slope on the southern crane pad prior to use of the pad by the crane. A letter must be provided to the Planning Department by the soils engineer stating that cut slope will not affect the overall stability of the area and is appropriate for the intended use.
 - D. Final grading for the southern crane pad shall be performed under the observation of a geotechnical engineer. A final letter from the geotechnical engineer shall be submitted to the Planning Department which states that the final grading is stable and non-erosive. Compaction reports shall be submitted to the Planning Department, which indicate a minimum of 90% relative compaction for all fill placement.
 - E. All inspections required by the grading permit shall be completed to the satisfaction of the County Senior Civil Engineer.
 - F. Pursuant to Sections 16.40.040 and 16.42.100 of the County Code, if at any time during site preparation, excavation, or other ground disturbance associated with this development, any artifact or other evidence of an historic archaeological resource or a Native American cultural site is discovered, the responsible persons shall immediately cease and desist from all further site excavation and notify the Sheriff-Coroner if the discovery contains human remains, or the Planning Director if the discovery contains no human remains. The procedures established in Sections 16.40.040 and 16.42.100, shall be observed.

V. Following construction, yearly re-vegetation monitoring reports shall be submitted annually to the Planning Department for review until re-vegetation plants achieve an 80 percent survival rate. An at-cost account with the County shall remain funded for review of the report until the end of the monitoring. Yearly monitoring reports shall be prepared by the re-vegetation specialist.

VI. Operational Conditions

A. In the event that future County inspections of the subject property disclose noncompliance with any Conditions of this approval or any violation of the County Code, the owner shall pay to the County the full cost of such County inspections, including any follow-up inspections and/or necessary enforcement actions, up to and including permit revocation.

VII. As a condition of this development approval, the holder of this development approval ("Development Approval Holder"), is required to defend, indemnify, and hold harmless the COUNTY, its officers, employees, and agents, from and against any claim (including attorneys' fees), against the COUNTY, its officers, employees, and agents to attack, set aside, void, or annul this development approval of the COUNTY or any subsequent amendment of this development approval which is requested by the Development Approval Holder.

A. COUNTY shall promptly notify the Development Approval Holder of any claim, action, or proceeding against which the COUNTY seeks to be defended, indemnified, or held harmless. COUNTY shall cooperate fully in such defense. If COUNTY fails to notify the Development Approval Holder within sixty (60) days of any such claim, action, or proceeding, or fails to cooperate fully in the defense thereof, the Development Approval Holder shall not thereafter be responsible to defend, indemnify, or hold harmless the COUNTY if such failure to notify or cooperate was significantly prejudicial to the Development Approval Holder.

B. Nothing contained herein shall prohibit the COUNTY from participating in the defense of any claim, action, or proceeding if both of the following occur:

1. COUNTY bears its own attorney's fees and costs; and
2. COUNTY defends the action in good faith.

C. Settlement. The Development Approval Holder shall not be required to pay or perform any settlement unless such Development Approval Holder has approved the settlement. When representing the County, the Development Approval Holder shall not enter into any stipulation or settlement modifying or affecting the interpretation or validity of any of the terms or conditions of the development approval without the prior written consent of the County.

D. Successors Bound. "Development Approval Holder" shall include the applicant and the successor(s) in interest, transferee(s), and assign(s) of the applicant.

Minor variations to this permit which do not affect the overall concept or density may be approved by the Planning Director at the request of the applicant or staff in accordance with Chapter 18.10 of the County Code.

Please note: This permit expires three years from the effective date listed below unless a grading permit (or permits) is obtained for the primary structure described in the development permit (does not include demolition, temporary power pole or other site preparation permits, or accessory structures unless these are the primary subject of the development permit). Failure to exercise the building permit and to complete all of the construction under the building permit, resulting in the expiration of the building permit, will void the development permit, unless there are special circumstances as determined by the Planning Director.

Approval Date: _____

Effective Date: _____

Expiration Date: _____

Wanda Williams
Deputy Zoning Administrator

Sheila McDaniel
Project Planner

Appeals: Any property owner, or other person aggrieved, or any other person whose interests are adversely affected by any act or determination of the Zoning Administrator, may appeal the act or determination to the Planning Commission in accordance with chapter 18.10 of the Santa Cruz County Code.

CALIFORNIA ENVIRONMENTAL QUALITY ACT NOTICE OF EXEMPTION

The Santa Cruz County Planning Department has reviewed the project described below and has determined that it is exempt from the provisions of CEQA as specified in Sections 15061 - 15332 of CEQA for the reason(s) which have been specified in this document.

Application Number: 141099
Assessor Parcel Number: 045-161-05
Project Location: No site address

Project Description: Proposal to replace the La Selva Beach railroad trestle and to grade approximately 150 cubic yards of material for creation of temporary access for a crane pad during the construction period on APNs 045-161-05 and 045-201-16 and to install a temporary sheet pile wall and crane pad on APN 045-022-04.

Person or Agency Proposing Project: Stacy and Witbeck Inc, Attention: Michael Dorsa

Contact Phone Number: (510) 748-1870

- A. The proposed activity is not a project under CEQA Guidelines Section 15378.
- B. The proposed activity is not subject to CEQA as specified under CEQA Guidelines Section 15060 (c).
- C. **Ministerial Project** involving only the use of fixed standards or objective measurements without personal judgment.
- D. **Statutory Exemption** other than a Ministerial Project (CEQA Guidelines Section 15260 to 15285).
- E. **Categorical Exemption**

Specify type: Class 1 15301. EXISTING FACILITIES

F. Reasons why the project is exempt:

Rehabilitation of an existing trestle in an area designated for rail transportation is allowed under the two following exemptions:

(c) Existing highways and streets, sidewalks, gutters, bicycle and pedestrian trails, and similar facilities (this includes road grading for the purpose of public safety).

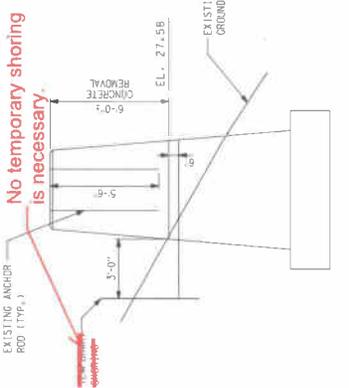
(d) Restoration or rehabilitation of deteriorated or damaged structures, facilities, or mechanical equipment to meet current standards of public health and safety.

In addition, none of the conditions described in Section 15300.2 apply to this project.

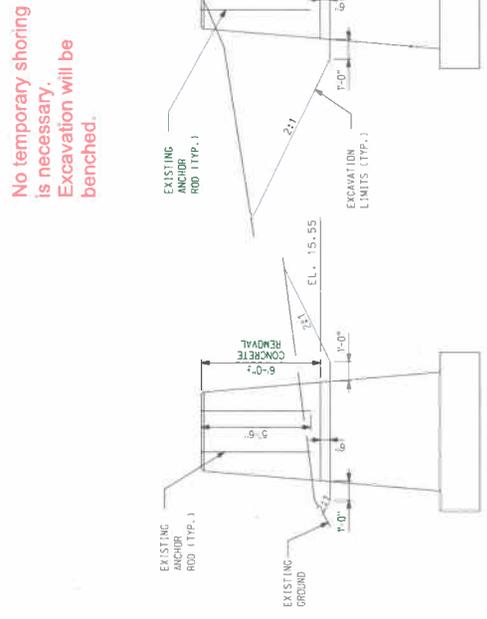
Sheila McDaniel, Project Planner

Date: _____

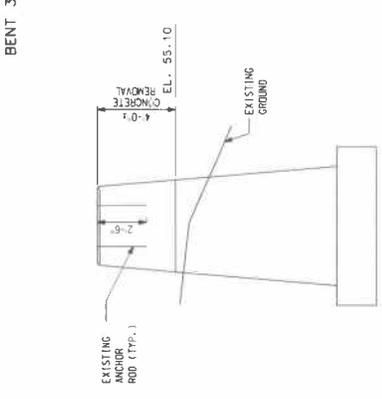




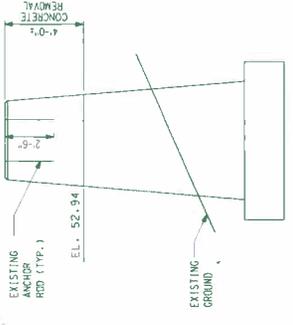
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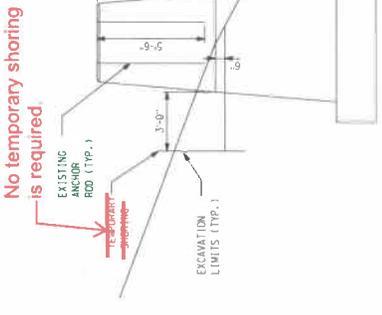
BENT 4



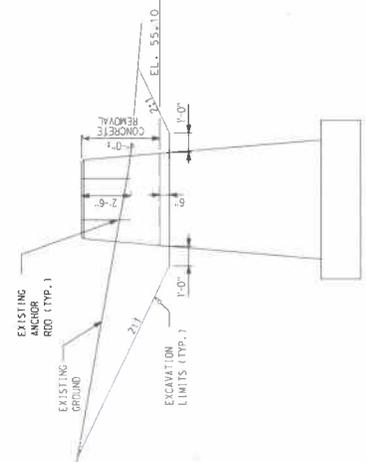
BENT 7



BENT 2



BENT 6



BENT 8



4/1/13

- NOTES:
- FOR GENERAL NOTES, SEE DRAWING NOS. 3 - 6.
 - TEMPORARY SHORING SHALL BE USED AT LOCATIONS AS SHOWN. TEMPORARY SHORING SHALL BE REMOVED UPON COMPLETION OF WORK.

FOUNDATION EXCAVATION
SCALE: 1" = 10'

CONFORMED

PROJECT NO.	LA SELVA DECK PLATE GIRDER	PROJECT NO.	109-09 - S13
DATE	APRIL 1, 2013	DRAWING NO.	13 OF 67
DESIGNED BY	JRN	SCALE	1" = 10'
DRAWN BY	AJK	DATE	APRIL 1, 2013
CHECKED BY	TER	DATE	APRIL 1, 2013
APPROVED BY	DES	DATE	APRIL 1, 2013
DESCRIPTION	FOUNDATION EXCAVATION AND REMOVALS		



DESIGNED BY: JRN
DRAWN BY: AJK
CHECKED BY: TER
APPROVED BY: DES
DATE: APRIL 1, 2013

LA SELVA DECK PLATE GIRDER
MILEPOST 9.09
FOUNDATION EXCAVATION AND REMOVALS

PROJECT NO. 109-09 - S13
DRAWING NO. 13 OF 67

DATE: APRIL 1, 2013

DESCRIPTION: FOUNDATION EXCAVATION AND REMOVALS

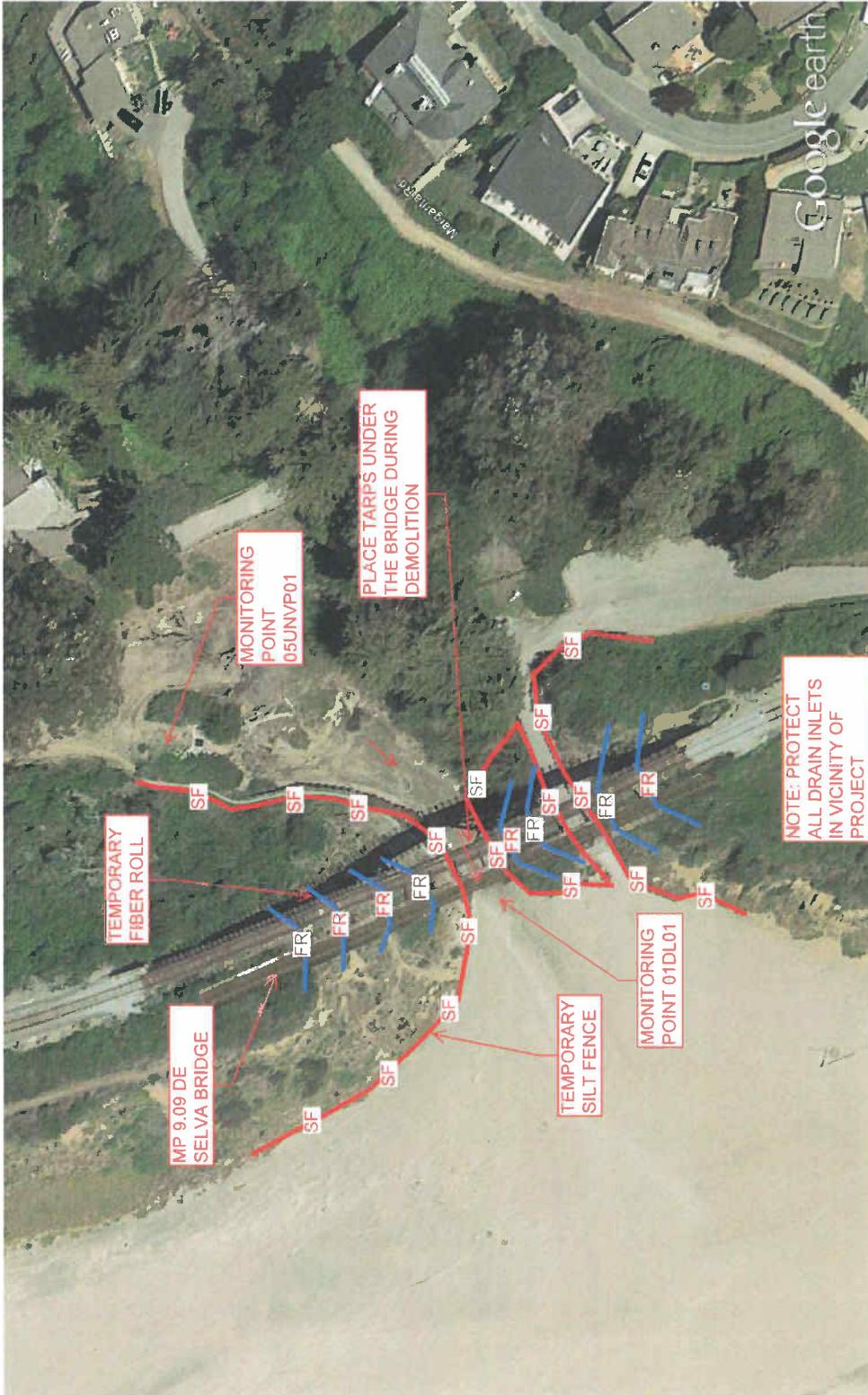
SCALE: 1" = 10'

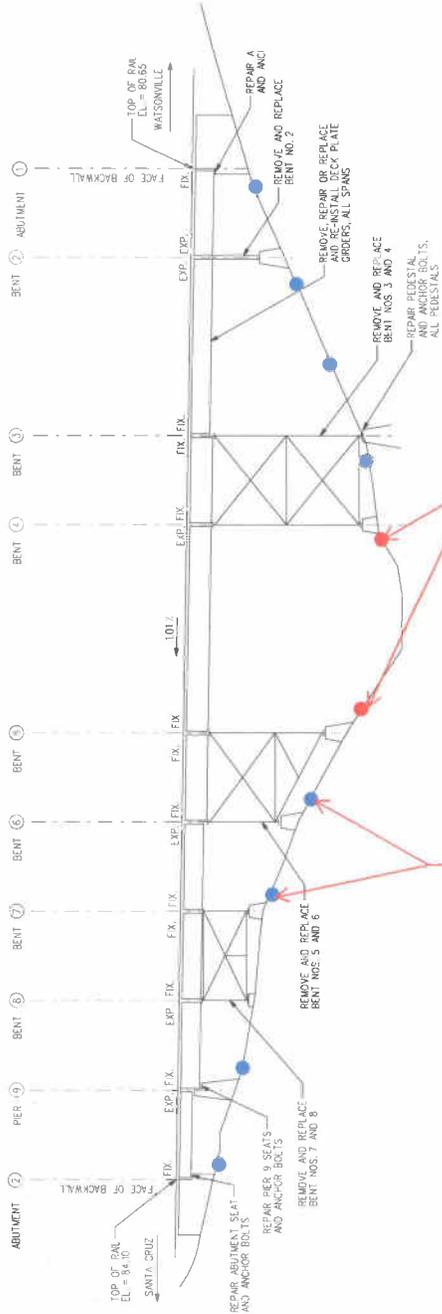
EXHIBIT D



WATER POLLUTION CONTROL NOTES:

1. PARK PAVING EQUIPMENT ON PLASTIC WHEN NOT IN USE AFTER WORK HOURS.
2. CAPTURE ALL CONCRETE AND ASPHALT RUBBLE AND ALL CONCRETE WASHOUT. PROPERLY DISPOSE OF IN AN APPROVED LANDFILL.
3. PROVIDE TEMPORARY COVER OVER STOCKPILES OR EXPOSED SOIL AREAS PRIOR TO RAINS OR IF LEFT INACTIVE OVER 14 DAYS.
4. PROVIDE WIND EROSION CONTROL AT ALL TIMES.
5. USE PERIMETER CONTROL SUCH AS FIBER ROLLS OR SILT FENCE TO CONTAIN DISTURBED SOIL AREAS TO PROTECT FROM RUN-ON AND RUN-OFF. USE GRAVEL BAGS AS NEEDED FOR SMALL AREAS OR ON PAVEMENT.
6. STREET SWEEP AS NEEDED AT ALL PAVED AREAS NEXT TO WORK.
7. ALL WASTE SHALL BE PICKED UP DAILY AND PLACED IN A COVERED WASTE BIN OR DUMPSTER OR HAULED OFF DAILY. DEMOLITION WASTE CAN BE PLACED IN A STOCKPILE PROVIDED IT IS COVERED BY END OF SHIFT AND REMOVED AT LEAST WEEKLY.
8. PROTECT ALL DRAIN INLETS IN VICINITY OF WORK.
9. PROVIDE EFFECTIVE EROSION CONTROL ON DISTURBED SOIL AREAS SUCH AS EROSION CONTROL BLANKETS OR TEMPORARY COVER PRIOR TO RAINS OR IF LEFT INACTIVE OVER 14 DAYS.
10. AT THE COMPLETION OF CONSTRUCTION, SPRAY EROSION CONTROL HYDROSEED ON ALL EXPOSED SOIL FOR PERMANENT EROSION CONTROL PER PROJECT SPECIFICATIONS.
11. PLACE TARPS UNDER THE BRIDGES DURING DEMOLITION.

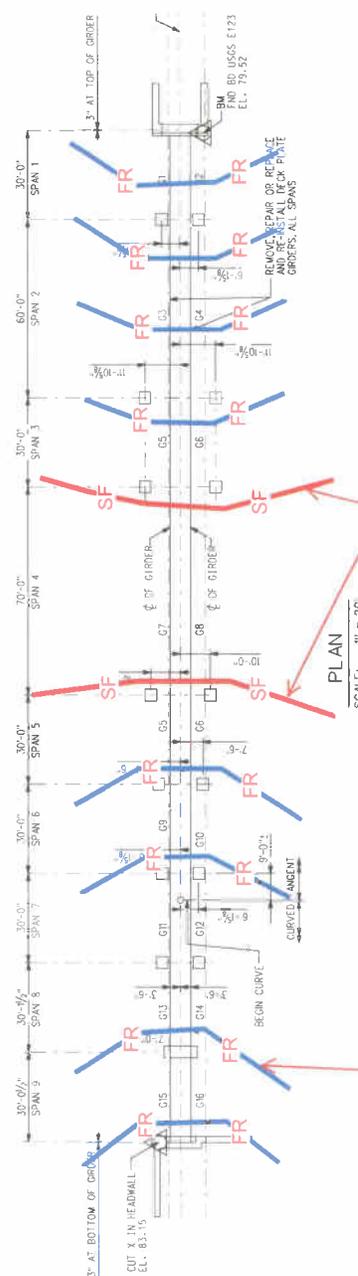




TEMPORARY SILT FENCE

TEMPORARY FIBER ROLL

ELEVATION
SCALE: 1" = 20'



SEE SHEET 277 FOR TEMPORARY SILT FENCE PLACEMENT.

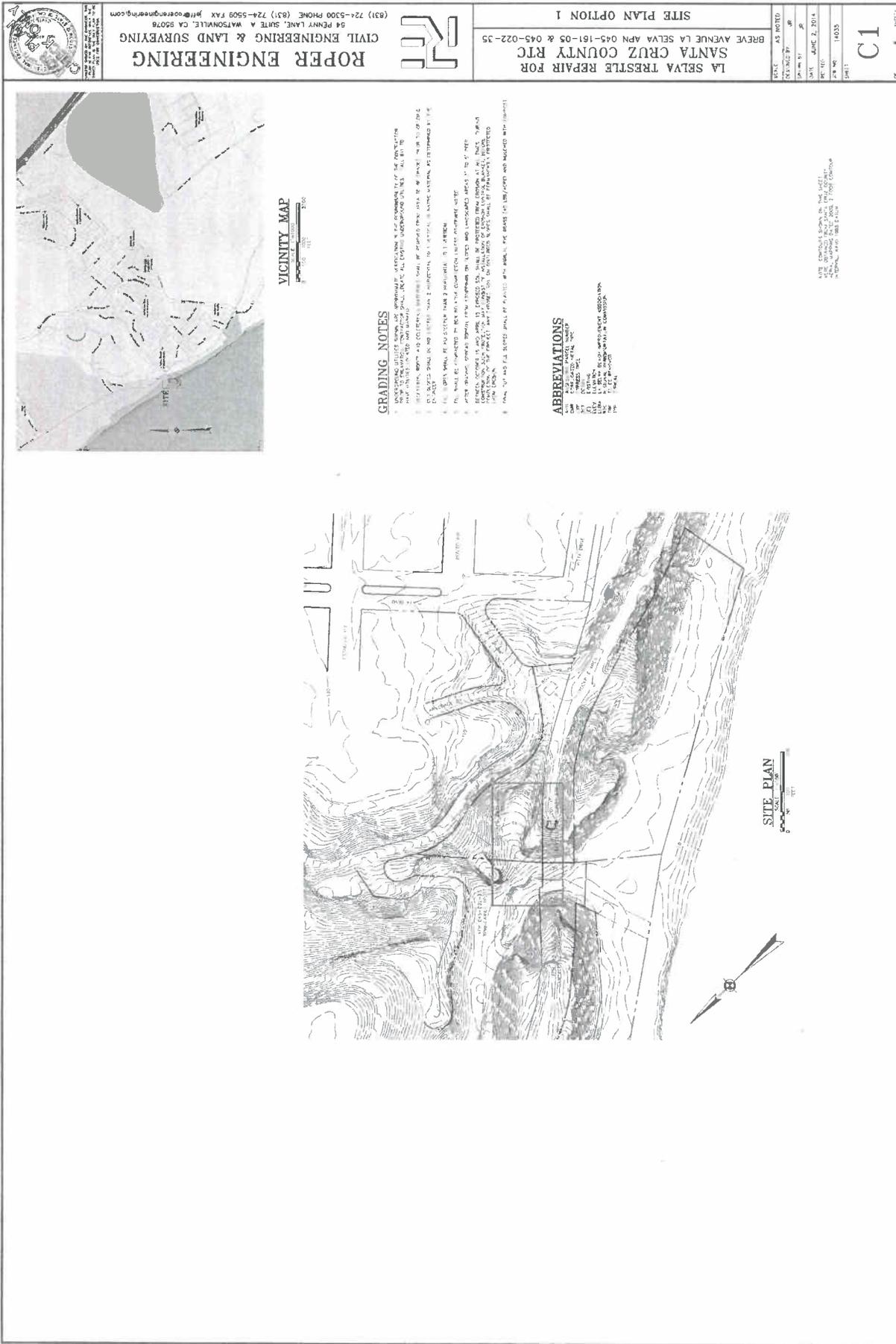
TEMPORARY FIBER ROLL

PLAN
SCALE: 1" = 20'

PROJECT NUMBER	20130101
DATE	4/27/13
DESIGNER	Z. P. ...
CHECKER	...
DATE	3/24/14
PROJECT	20130101

EXHIBIT D

TULLY CONSULTING GROUP
TEMPORARY WATER POLLUTION CONTROL
DRAWINGS
01/14/14



DATE: 06/11/10
 DRAWN BY: JACOB J. ZOU
 CHECKED BY: JACOB J. ZOU
 SCALE: 1" = 40'

ABBREVIATIONS
 ALL DIMENSIONS UNLESS OTHERWISE NOTED ARE IN FEET AND DECIMALS THEREOF.
 ALL DIMENSIONS SHALL BE TO THE CENTERLINE UNLESS OTHERWISE NOTED.
 ALL DIMENSIONS SHALL BE TO THE FACE UNLESS OTHERWISE NOTED.
 ALL DIMENSIONS SHALL BE TO THE CENTERLINE UNLESS OTHERWISE NOTED.
 ALL DIMENSIONS SHALL BE TO THE FACE UNLESS OTHERWISE NOTED.

GRADING NOTES
 1. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 2. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 3. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 4. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 5. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 6. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 7. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.
 8. ALL GRADING SHALL BE TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.

VICINITY MAP
 SCALE: 1" = 100'

<p>LA SELVA TRESTLE REPAIR FOR SANTA CRUZ COUNTY RTC BREVE AVENUE LA SELVA APN 045-161-05 & 045-022-35</p>		<p>SCALE: AS NOTED DATE: JUNE 2, 2010 DRAWN BY: JACOB J. ZOU CHECKED BY: JACOB J. ZOU PROJECT NO: 14033</p>
<p>ROPER ENGINEERING & LAND SURVEYING CIVIL ENGINEERING & LAND SURVEYING 54 PENNY LANE, SUITE A WATSONVILLE, CA 95076 (831) 724-5300 PHONE (831) 724-5509 FAX jzou@roperengineering.com</p>		<p>C1</p>



ROPER ENGINEERING
 CIVIL ENGINEERING & LAND SURVEYING
 64 PENNY LANE, SUITE A WATSONVILLE, CA 95076
 (831) 724-5000 P-ONE (831) 724-5509 FAX rj@roperengineering.com

LA SELVA TRESTLE REPAIR FOR SANTA CRUZ COUNTY RTC
 BREVE AVENUE LA SELVA APN 045-161-05 & 045-022-35
 FINAL GRADING PLAN OPTION 1

DATE	AS NOTED
SCALE	AS SHOWN
DATE	JULY 2, 2014
PROJECT NO.	14005
PROJECT NAME	LA SELVA TRESTLE REPAIR FOR SANTA CRUZ COUNTY RTC

C4



**EARTH RETENTION SYSTEM
 SANTA CRUZ BRANCH RAIL IMPROVEMENT
 LA SELVA BRIDGE NORTH CRANE PAD
 FOR SUPERSTRUCTURE ERECTION
 SANTA CRUZ, CA**

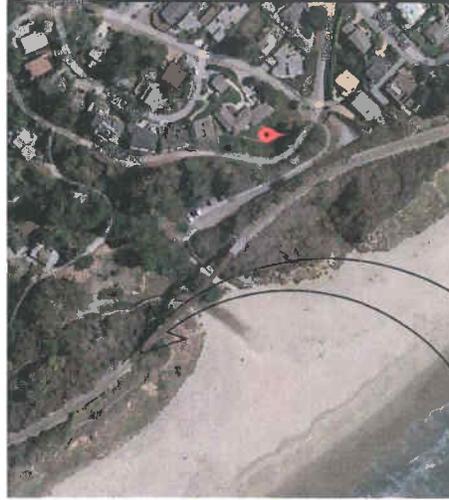
STACY AND WITBECK, INC.
 2800 HARBOR BAY PARKWAY
 ALAMEDA, CA 94502
 TEL: (510) 748-1870
 FAX: (510) 748-1205



PB&A, INC.
 818 FIFTH AVE., SUITE 207
 SAN RAFAEL, CA 94901
 TEL: (415) 259-0191
 FAX: (415) 223-8589



JOB NO. 140100 MAY 21, 2014



PROJECT LOCATION

SITE LOCATION PLAN



GENERAL

- ALL CONSTRUCTION SHALL CONFORM TO:
 - CALTRANS BRACING AND SHORING MANUAL (REV 01, AUGUST 2011)
 - CAL/OSHA TECHNICAL MANUAL SECTION V CHAPTER 2: EXCAVATION
 - CALIFORNIA BUILDING CODE, 2010
- THESE DRAWINGS ARE BASED ON THE INFORMATION PROVIDED FROM THE FOLLOWING DOCUMENTS:
 - FOUNDATION PLANS FOR LEONARD GULCH MADUOT PROJECT PREPARED BY SOUTHERN PACIFIC COMPANY DATED 10/17/1926.
 - ERECTION PLAN - ALTERNATE METHOD PREPARED BY DANNY'S CONSTRUCTION CO. INC. DATED 03/20/2014.
 - CONSTRUCTION MAP PROVIDED BY PARKH CONSULTANTS INC DATED 08/29/2012.
 - MANUFACTURER'S SPECIFICATION FOR LIEBHERR LTM 1220-5.2 MOBILE CRANE.

THE DATA FOR THE ALIGNMENT OF WALL, ITS GEOMETRY AND ELEVATIONS ARE FOR REFERENCE ONLY AND DO NOT CONSTITUTE A DESIGN. THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND ELEVATIONS ARE FOR REFERENCE ONLY AND DO NOT CONSTITUTE A DESIGN. THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND ELEVATIONS ARE FOR REFERENCE ONLY AND DO NOT CONSTITUTE A DESIGN. THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND ELEVATIONS ARE FOR REFERENCE ONLY AND DO NOT CONSTITUTE A DESIGN. THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND ELEVATIONS ARE FOR REFERENCE ONLY AND DO NOT CONSTITUTE A DESIGN.

MATERIALS

- STRUCTURAL STEEL FOR SHEET PILE SHALL CONFORM TO ASTM A572 GR. 50.
- STRUCTURAL STEEL FOR MISCELLANEOUS ITEMS SUCH AS PLATE, ANGLE IRON SHALL CONFORM TO ASTM A36 GR. 36.
- ANCHOR BOLT SHALL CONFORM TO ASTM A563 CLASS C.
- NUTS SHALL CONFORM TO ASTM A563 GRADE A, HEX.
- WASHERS SHALL CONFORM TO ASTM F844.
- CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIALS.
- TIMBER MAT & LAGGING SHALL BE DF# 2.
- BACKFILL SHALL BE GRANULAR FILL FREE OF COHESIVE MATERIAL, WHEN FOLLOWS: ACCORDANCE W/ AASHTO T 289, THE GRADE SHALL BE AS FOLLOWS:
 - 100 TO 75 PERCENT PASSING A 2-INCH SIEVE
 - 100 TO 75 PERCENT PASSING A 3/4-INCH SIEVE
 - 100 TO 20 PERCENT PASSING A NO. 4 SEIVE
 - 100 TO 10 PERCENT PASSING A NO. 20 SEIVE
 - 0 TO 35 PERCENT PASSING A NO. 200 SEIVE

THE DESIGN OF THE EARTH RETENTION SYSTEM IS BASED ON THE BORING LOGS SHOWN IN THE FINAL REPORT FOR LA SELVA BEACH TRESTLE STRUCTURE RETROFIT PROJECT BY PARKH CONSULTANTS INC., DATED DECEMBER 14, 2012.

CONSTRUCTION PROCEDURE FOR SHEET PILE SHORING

- REMOVE THE PLANTATION AND BOULDER WITHIN THE PROJECTED AREA OF THE SUBJECT CRANE PAD. THE EXISTING GRADE SHALL BE CLEANED OFF ANY DEBRIS AND LOOSE MATERIALS.
- CONFIRM THE EXISTING GRADE AS SHOWN ON PLAN WITH EDR.
- DRIVE THE SHEET PILE TO THE LENGTH AND DEPTH AS INDICATED ON PLAN.
- BACKFILL BEHIND THE SHEET PILE TO THE TARGET ELEVATION. THE LATERAL PRESSURE ON THE SHEET PILE WALL CAUSED BY COMPACTION SHALL NOT EXCEED 200 PSF.
- PLACE THE TIMBER MATS AT THE LOCATIONS AS SHOWN ON PLAN. THE SUBJECT CRANE OUTRIGGER SHALL BE CENTERED ON THE TIMBER MATS DURING OPERATION.

THE DESIGN OF THE EARTH RETENTION SYSTEM IS BASED ON THE BORING LOGS SHOWN IN THE FINAL REPORT FOR LA SELVA BEACH TRESTLE STRUCTURE RETROFIT PROJECT BY PARKH CONSULTANTS INC., DATED DECEMBER 14, 2012.

DESIGN CRITERIA

- INTERNAL FRICTION ANGLE, $\phi = 38^\circ$
- ACTIVE EARTH PRESSURE COEFFICIENT $K_a = 0.3$, EQUIVALENT FLUID PRESSURE = 35 PCF
- PASSIVE EARTH PRESSURE COEFFICIENT $K_p = 4.2$, EQUIVALENT FLUID PRESSURE = 500 PCF
- SOIL UNIT WEIGHT, $\gamma = 120$ PCF



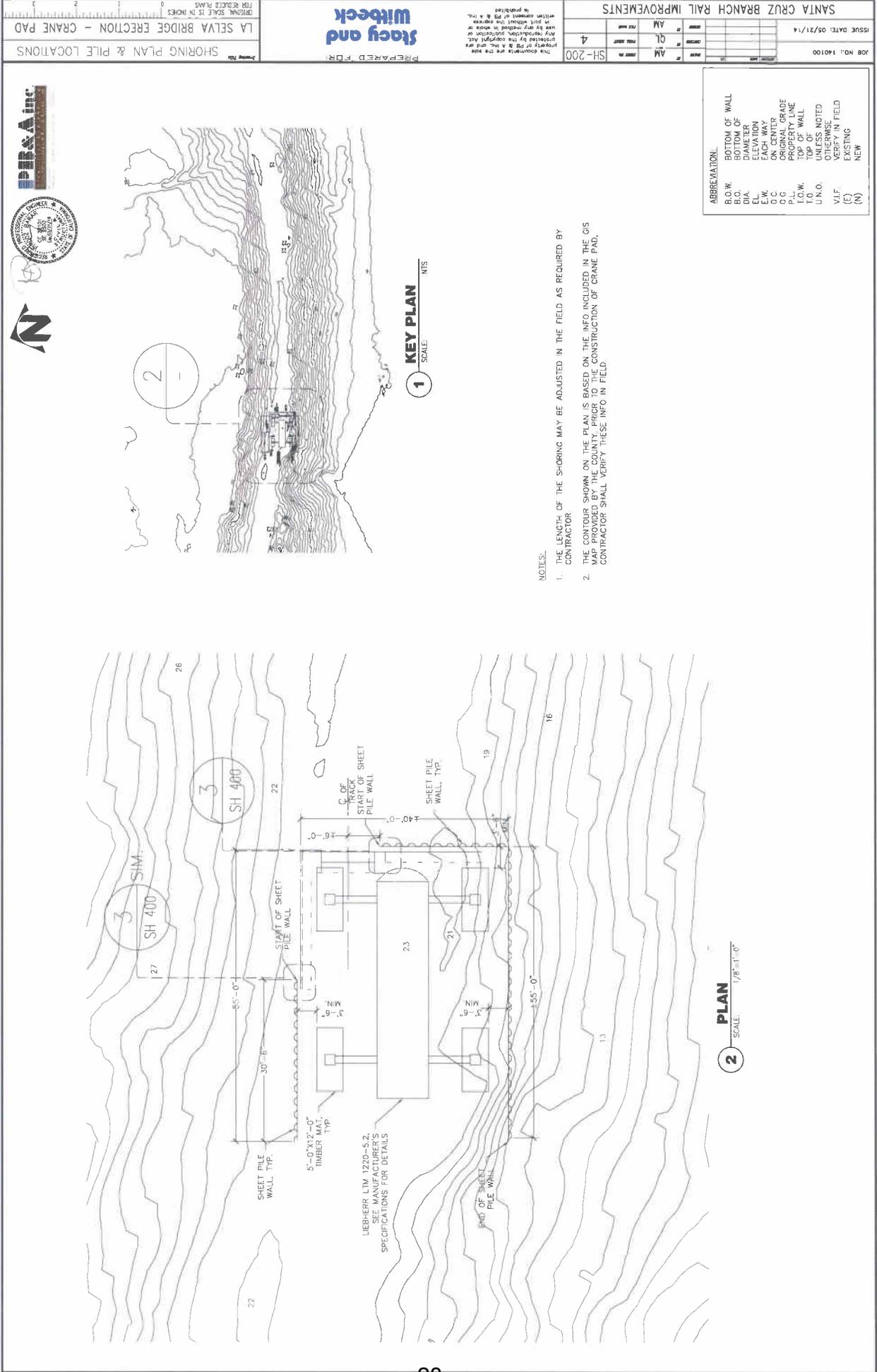
MONITORING

- CONTRACTOR SHALL MONITOR OF MOVEMENT AND SETTLEMENT OF WALL AS WELL AS THE SURROUNDING STRUCTURES DURING CONSTRUCTION.
- MONITORING POINTS ARE TO BE ESTABLISHED AT THE PERIMETER OF CRANE PAD INCLUDING THE SHEET PILE WALL AND THE EXISTING BRIDGE ABUTMENT.
- MONITORING POINTS ARE NOT TO BE MORE THAN 50 FEET APART AND ARE TO BE ESTABLISHED AT THE PERIMETER OF CRANE PAD. THE FREQUENCY OF DURATION OF CONSTRUCTION FREQUENCY MAY BE INCREASED OR DECREASED AT THE DISCRETION OF SHORING ENGINEER.



DRAWING LIST

- SH-100 GENERAL NOTES
- SH-200 KEY PLAN AND PARTIAL PLAN
- SH-300 ELEVATION
- SH-101 ELEVATION
- SH-100 SECTIONS & DETAIL



ABBREVIATION:

B.O.W.	BOTTOM OF WALL
B.O.	BOTTOM OF
ELEVATION	ELEVATION
E.W.	EACH WAY
O.C.	ON CENTER
O.G.	ORIGINAL GRADE
P.L.O.W.	PROPERTY LINE
T.O.	TOP OF
U.N.O.	UNLESS NOTED OTHERWISE
V.I.F.	VERIFY IN FIELD
(E)	EXISTING
(N)	NEW

- NOTES:
1. THE LENGTH OF THE SHORING MAY BE ADJUSTED IN THE FIELD AS REQUIRED BY CONTRACTOR
 2. THE CONTOUR SHOWN ON THE PLAN IS BASED ON THE INFO INCLUDED IN THE GIS MAP PROVIDED BY THE COUNTY. PRIOR TO THE CONSTRUCTION OF CRANE PAD, CONTRACTOR SHALL VERIFY THESE INFO IN FIELD.

1 KEY PLAN
SCALE: 1"=100'

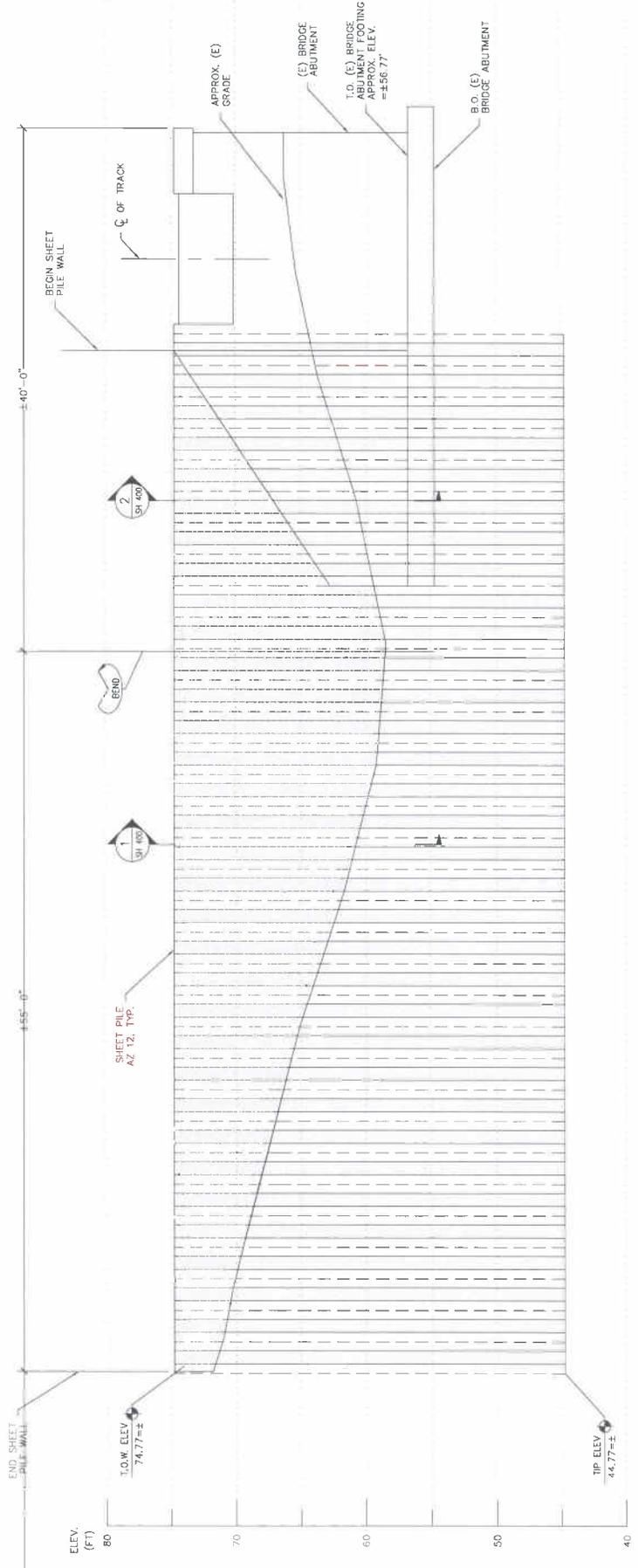
2 PLAN
SCALE: 1/8"=1'-0"

SHORING PLAN & PILE LOCATIONS
LA SELVA BRIDGE ERECTION - CRANE PAD
ORIGINAL SCALE IS 1/2"=1'-0"
THIS SCALE IS 1/2"=1'-0"

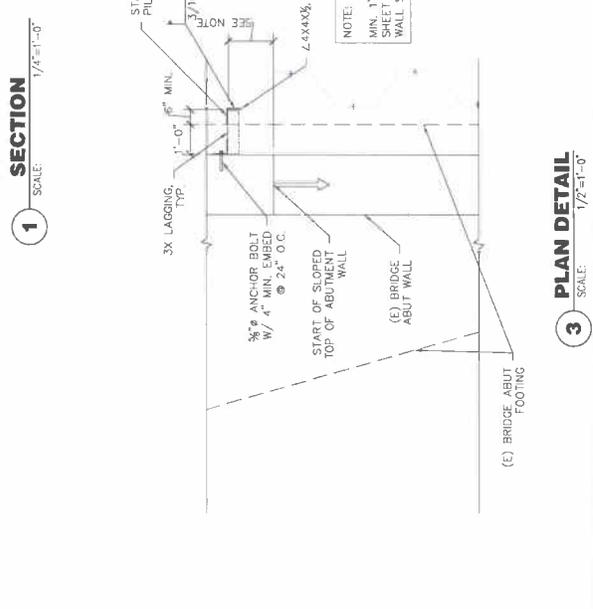
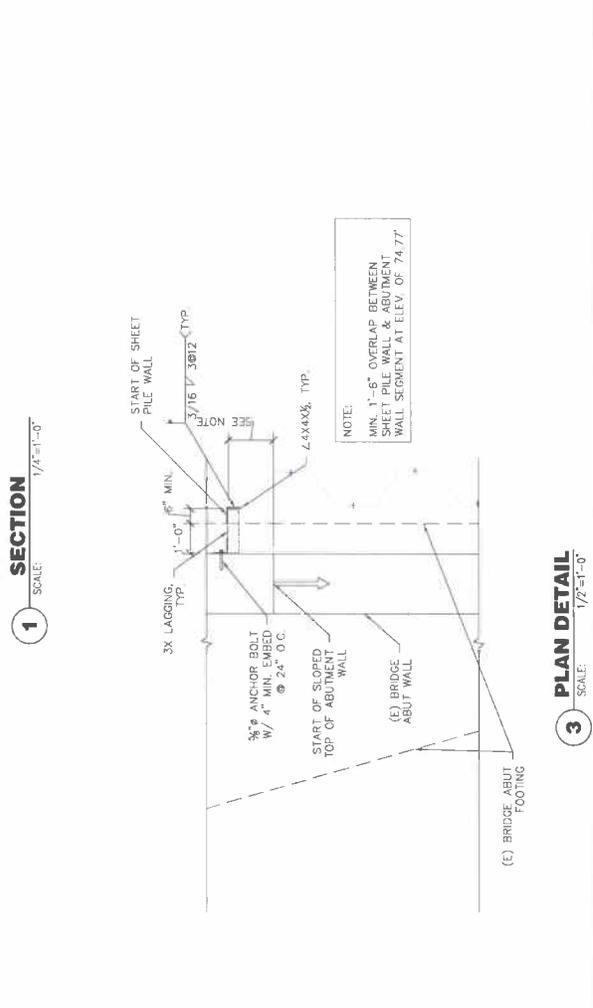
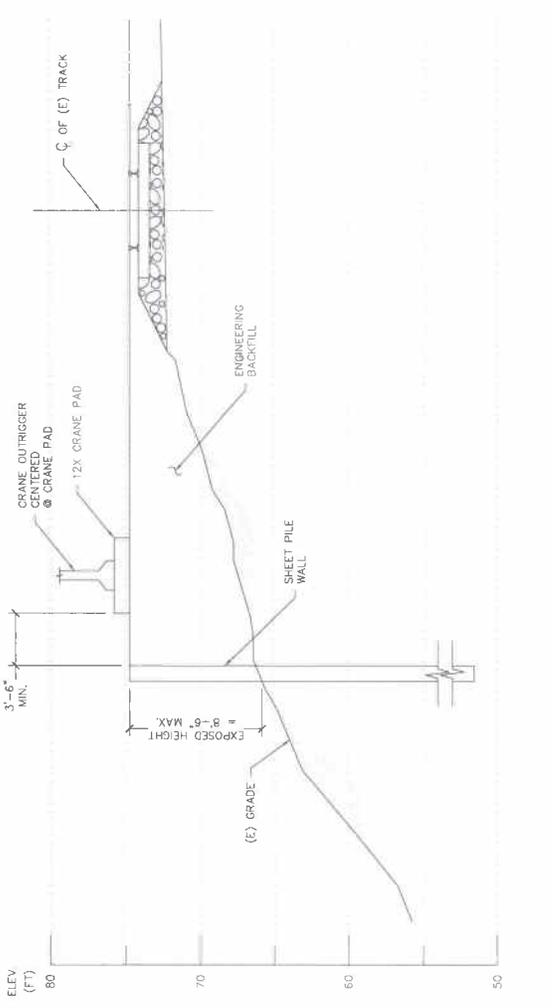
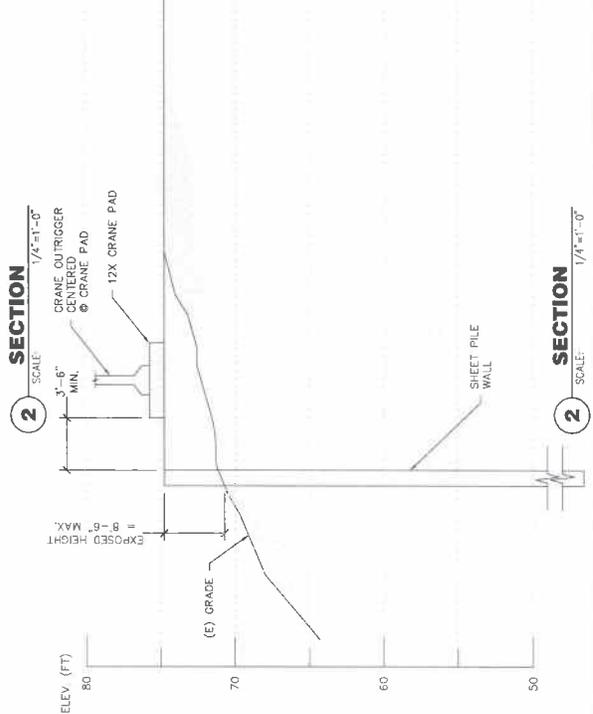
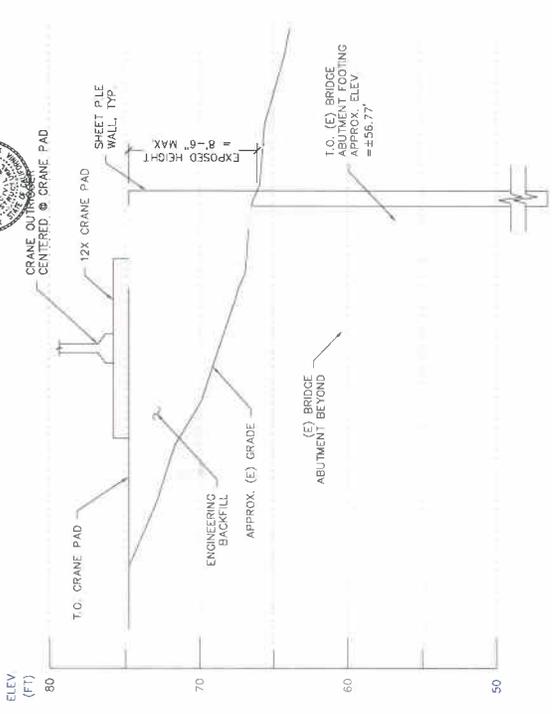
PREPARED FOR
Stacy and Witbeck

JOB NO.: 140100
ISSUE DATE: 05/21/14
SANTA CRUZ BRANCH RAIL IMPROVEMENTS
REV. NO. 4
REV. DATE 4/14/14
REV. BY AM





1 ELEVATION
 SCALE: 1/4"=1'-0"



**EARTH RETENTION SYSTEM
SANTA CRUZ BRANCH RAIL IMPROVEMENT
LA SELVA BRIDGE NORTH CRANE PAD
FOR SUPERSTRUCTURE ERECTION
SANTA CRUZ, CA**

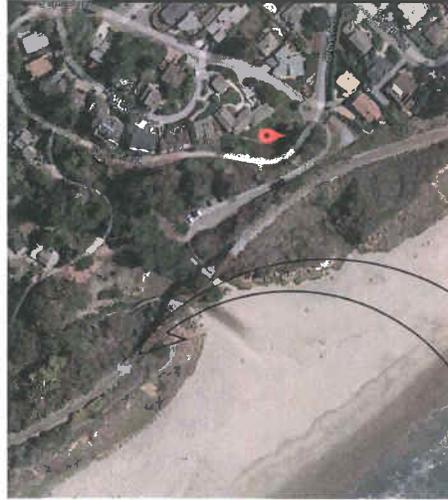
STACY AND WITBECK, INC.
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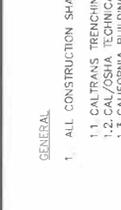
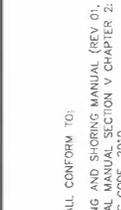


JOB NO. 140100 MAY 21, 2014



PROJECT LOCATION

SITE LOCATION PLAN



MATERIALS

- STRUCTURAL STEEL FOR SHEET PILE SHALL CONFORM TO ASTM A572 GR. 50.
- STRUCTURAL STEEL FOR MISCELLANEOUS ITEMS SUCH AS PLATE, ANGLE IRON SHALL CONFORM TO ASTM A36 GR. 36.
- ANCHOR BOLT SHALL CONFORM TO ASTM A153 CLASS C.
- NUTS SHALL CONFORM TO ASTM A563 GRADE A, HEX.
- WASHERS SHALL CONFORM TO ASTM FB44.
- CONTRACTOR SHALL VERIFY ALL CONTROLLING FIELD DIMENSIONS BEFORE ORDERING OR FABRICATING ANY MATERIALS.
- TIMBER MAT & LAGGING SHALL BE DF # 2.
- BACKFILL SHALL BE GRANULAR FILL FREE OF COHESIVE MATERIAL WHEN TESTED IN ACCORDANCE W/ ASTM D 1557. THE GRADE SHALL BE AS FOLLOWS:
 - A. 100 TO 75 PERCENT PASSING A 2-INCH SIEVE
 - B. 100 TO 75 PERCENT PASSING A 3/4-INCH SIEVE
 - C. 100 TO 60 PERCENT PASSING NO. 40 SIEVE
 - D. 100 TO 60 PERCENT PASSING NO. 20 SIEVE
 - E. 0 TO 35 PERCENT PASSING A NO. 200 SIEVE

CONSTRUCTION PROCEDURE FOR SHEET PILE SHORING

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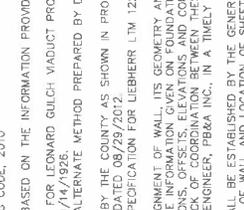
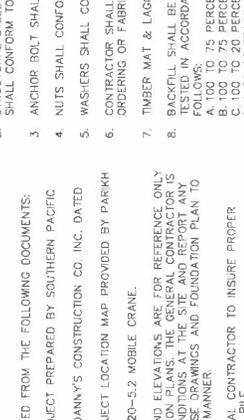
MONITORING

- CONTRACTOR SHALL MONITOR OF MOVEMENT AND SETTLEMENT OF WALL AS WELL AS THE SURROUNDING STRUCTURES DURING CONSTRUCTION.
- MONITORING POINTS ARE TO BE ESTABLISHED AT THE PERIMETER OF CRANE PAD INCLUDING THE SHEET PILE WALL AND THE EXISTING BRIDGE ABUTMENT.
- MONITORING POINTS ARE NOT TO BE MORE THAN 50 FEET APART AND ARE TO BE SURVEYED AT A FREQUENCY OF AT LEAST ONCE A WEEK FOR THE DURATION OF CONSTRUCTION. FREQUENCY MAY BE INCREASED OR DECREASED AT THE DISCRETION OF SHORING ENGINEER.

DESIGN CRITERIA

THE DESIGN OF THE EARTH RETENTION SYSTEM IS BASED ON THE BORING LOGS SHOWN IN THE FINAL REPORT FOR THE LA SELVA BEACH TRISTLE STRUCTURE RETROFIT PROJECT BY PARKHI CONSULTANTS INC., DATED DECEMBER 14, 2012.

PRESSURE DIAGRAM FOR CANTILEVER SHORING
 ACTIVE EARTH PRESSURE COEFFICIENT $K_a = 0.3$, EQUIVALENT FLUID PRESSURE = 35 PCF
 PASSIVE EARTH PRESSURE COEFFICIENT $K_p = 4.2$, EQUIVALENT FLUID PRESSURE = 500 PCF
 SOIL UNIT WEIGHT, $\gamma = 120$ PCF
 INTERNAL FRICTION ANGLE, $\phi = 35^\circ$



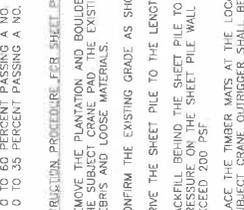
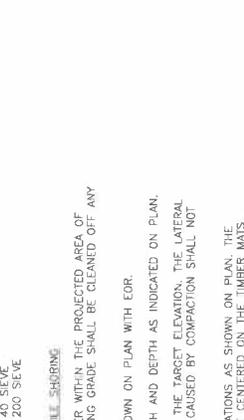
GENERAL

- ALL CONSTRUCTION SHALL CONFORM TO:
 - 1.1 CALTRANS TRENCHING AND SHORING MANUAL (REV 01, AUGUST 2011)
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 - A. FOUNDATION PLANS FOR LEONARD GULCH VIADUCT PROJECT PREPARED BY SOUTHERN PACIFIC COMPANY DATED 10/14/1926.
 - B. ERECTION PLAN - ALTERNATE METHOD PREPARED BY DANNY'S CONSTRUCTION CO. INC. DATED 02/20/12.
 - C. GSP PROVIDED BY THE COUNTY AS SHOWN IN PROJECT LOCATION MAP PROVIDED BY PARKHI CONSULTANTS INC. DATED 08/29/2012.
 - D. MANUFACTURER'S SPECIFICATION FOR LEBHERR LTM 11220-5.2 MOBILE CRANE.
- THE DATA FOR THE ALIGNMENT OF WALL, ITS GEOMETRY AND ELEVATIONS ARE FOR REFERENCE ONLY. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, OFFSETS, ELEVATIONS AND CONDITIONS AT THE SITE AND REPORT ANY DISCREPANCIES AND LACK OF COORDINATION BETWEEN THESE DRAWINGS AND FOUNDATION PLAN TO THE EARTH RETENTION ENGINEER, PB&A INC., IN A TIMELY MANNER.
- ALL LINES AND GRABBS SHALL BE ESTABLISHED BY THE GENERAL CONTRACTOR TO INSURE PROPER HORIZONTAL ALIGNMENT OF WALL AND LOCATION OF SHEET PILE.
- THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND OTHER OBSTACLES IN THE PROJECT AREA PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE TO POT-HOLE ALL UTILITIES (AS NEEDED) BEFORE SHORING WALL CONSTRUCTION IS TO BEGIN. GENERAL CONTRACTOR IS TO REPORT ALL INTERFERENCES BETWEEN THE UTILITIES OR OTHER OBSTACLE WITH THE SHEET PILE TO THE SHORING ENG., PB&A IN A TIMELY MANNER.

DESIGN CRITERIA

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PRESSURE DIAGRAM FOR CANTILEVER SHORING
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 SOIL UNIT WEIGHT, $\gamma = 120$ PCF
 INTERNAL FRICTION ANGLE, $\phi = 35^\circ$



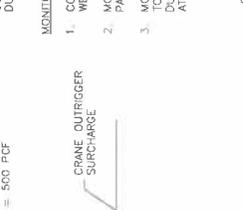
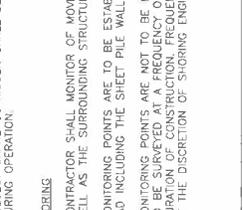
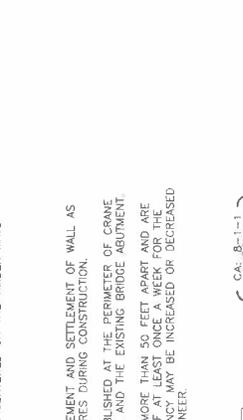
GENERAL NOTES

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DESIGN CRITERIA

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 PASSIVE EARTH PRESSURE COEFFICIENT $K_p = 4.2$, EQUIVALENT FLUID PRESSURE = 500 PCF
 SOIL UNIT WEIGHT, $\gamma = 120$ PCF
 INTERNAL FRICTION ANGLE, $\phi = 35^\circ$

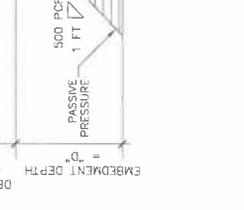
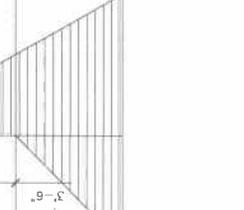
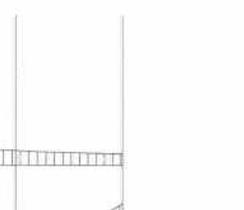


MONITORING

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- MONITORING POINTS ARE TO BE ESTABLISHED AT THE PERIMETER OF CRANE PAD INCLUDING THE SHEET PILE WALL AND THE EXISTING BRIDGE ABUTMENT.
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DRAWING LIST

- SH-100 GENERAL NOTES
- SH-200 KEY PLAN AND PARTIAL PLAN
- SH-300 ELEVATION
- SH-400 SECTIONS & DETAIL



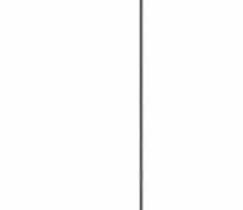
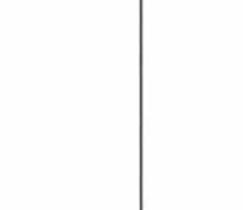
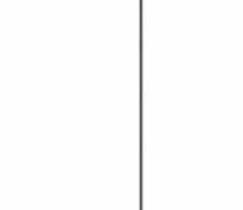
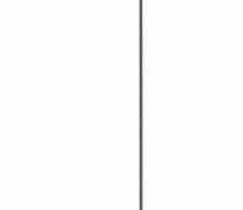
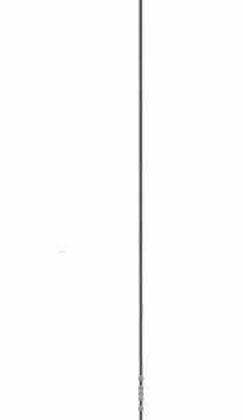
GENERAL NOTES

- ALL CONSTRUCTION SHALL CONFORM TO:
 - 1.1 CALTRANS TRENCHING AND SHORING MANUAL (REV 01, AUGUST 2011)
 - 1.2 CAL/OSHA TECHNICAL MANUAL SECTION V CHAPTER 2: EXCAVATION
 - 1.3 CALIFORNIA BUILDING CODE, 2010
- THESE DRAWINGS ARE BASED ON THE INFORMATION PROVIDED FROM THE FOLLOWING DOCUMENTS:
 - A. FOUNDATION PLANS FOR LEONARD GULCH VIADUCT PROJECT PREPARED BY SOUTHERN PACIFIC COMPANY DATED 10/14/1926.
 - B. ERECTION PLAN - ALTERNATE METHOD PREPARED BY DANNY'S CONSTRUCTION CO. INC. DATED 02/20/12.
 - C. GSP PROVIDED BY THE COUNTY AS SHOWN IN PROJECT LOCATION MAP PROVIDED BY PARKHI CONSULTANTS INC. DATED 08/29/2012.
 - D. MANUFACTURER'S SPECIFICATION FOR LEBHERR LTM 11220-5.2 MOBILE CRANE.
- THE DATA FOR THE ALIGNMENT OF WALL, ITS GEOMETRY AND ELEVATIONS ARE FOR REFERENCE ONLY. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, OFFSETS, ELEVATIONS AND CONDITIONS AT THE SITE AND REPORT ANY DISCREPANCIES AND LACK OF COORDINATION BETWEEN THESE DRAWINGS AND FOUNDATION PLAN TO THE EARTH RETENTION ENGINEER, PB&A INC., IN A TIMELY MANNER.
- ALL LINES AND GRABBS SHALL BE ESTABLISHED BY THE GENERAL CONTRACTOR TO INSURE PROPER HORIZONTAL ALIGNMENT OF WALL AND LOCATION OF SHEET PILE.
- THE GENERAL CONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND OTHER OBSTACLES IN THE PROJECT AREA PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE TO POT-HOLE ALL UTILITIES (AS NEEDED) BEFORE SHORING WALL CONSTRUCTION IS TO BEGIN. GENERAL CONTRACTOR IS TO REPORT ALL INTERFERENCES BETWEEN THE UTILITIES OR OTHER OBSTACLE WITH THE SHEET PILE TO THE SHORING ENG., PB&A IN A TIMELY MANNER.

DESIGN CRITERIA

THE DESIGN OF THE EARTH RETENTION SYSTEM IS BASED ON THE BORING LOGS SHOWN IN THE FINAL REPORT FOR THE LA SELVA BEACH TRISTLE STRUCTURE RETROFIT PROJECT BY PARKHI CONSULTANTS INC., DATED DECEMBER 14, 2012.

PRESSURE DIAGRAM FOR CANTILEVER SHORING
 ACTIVE EARTH PRESSURE COEFFICIENT $K_a = 0.3$, EQUIVALENT FLUID PRESSURE = 35 PCF
 PASSIVE EARTH PRESSURE COEFFICIENT $K_p = 4.2$, EQUIVALENT FLUID PRESSURE = 500 PCF
 SOIL UNIT WEIGHT, $\gamma = 120$ PCF
 INTERNAL FRICTION ANGLE, $\phi = 35^\circ$



GENERAL NOTES

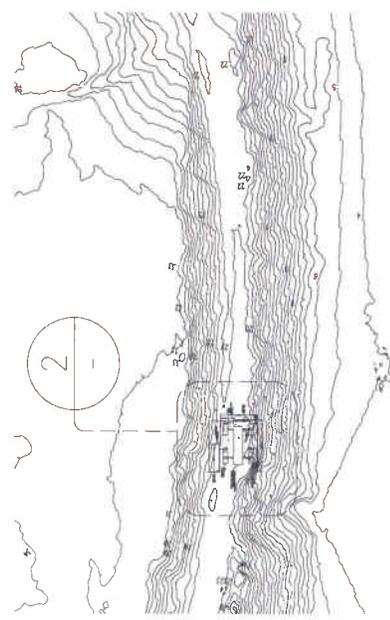
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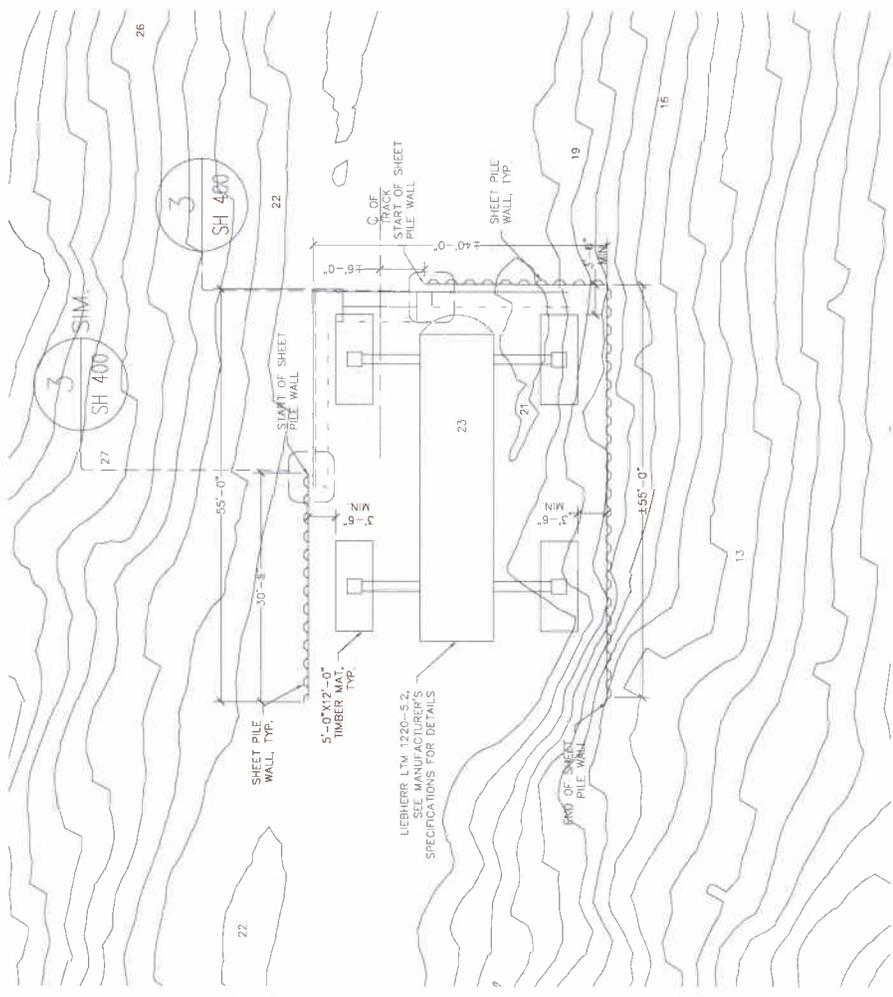


1 KEY PLAN
 SCALE: 1" = 10'

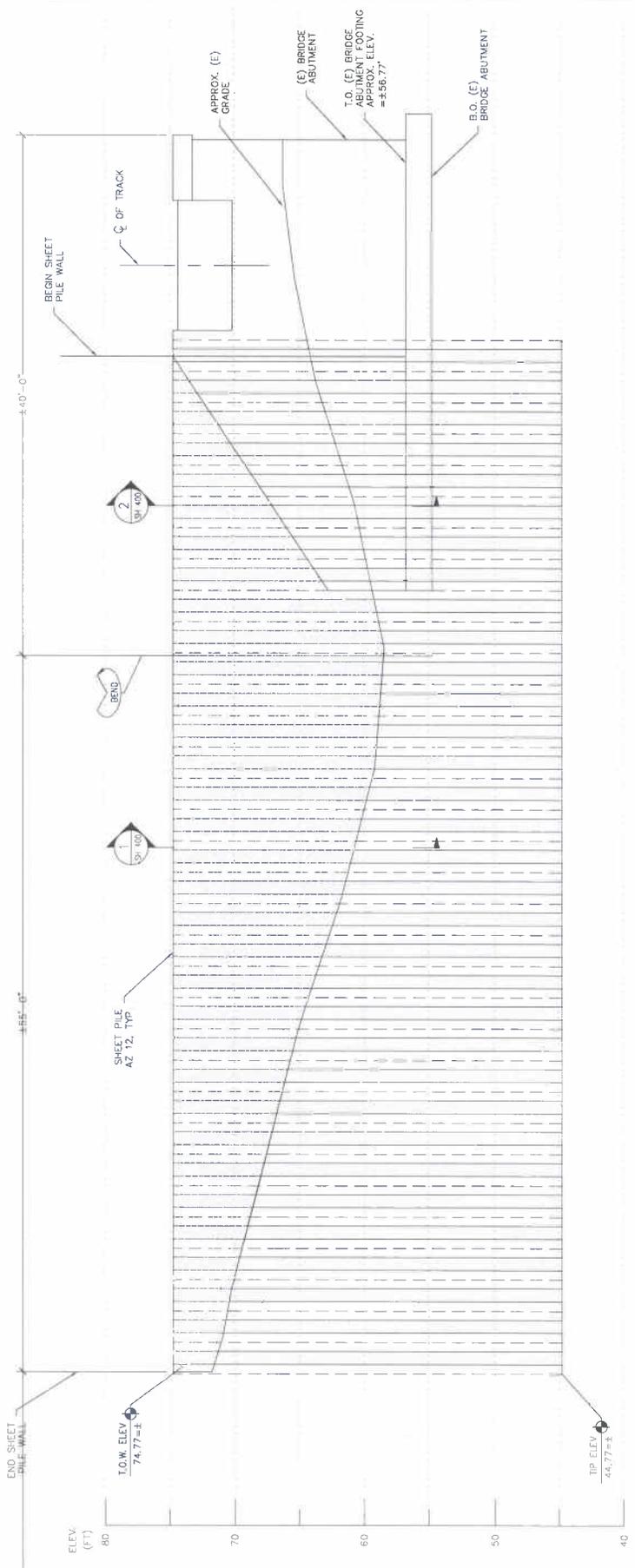
- NOTES:
1. THE LENGTH OF THE SHORING MAY BE ADJUSTED IN THE FIELD AS REQUIRED BY CONTRACTOR.
 2. THE CONTOUR SHOWN ON THE PLAN IS BASED ON THE INFO INCLUDED IN THE GIS MAP PROVIDED BY THE COUNTY, PRIOR TO THE CONSTRUCTION OF CRANE PAD. CONTRACTOR SHALL VERIFY THESE INFO IN FIELD.

ABBREVIATION:

B.O.W.	BOTTOM OF WALL
B.C.	BOTTOM OF CONCRETE
E.W.	EACH WAY
O.C.	ON CENTER
O.G.	ORIGINAL GRADE
P.O.L.	PROPRIETARY LINE
T.O.W.	TOP OF WALL
T.O.	TOP OF
U.N.O.	UNLESS NOTED OTHERWISE
V.I.F.	VERIFY IN FIELD
(C)	EXISTING
(N)	NEW



2 PLAN
 SCALE: 1/8" = 1'-0"



1 ELEVATION
 SCALE: 1/4" = 1'-0"



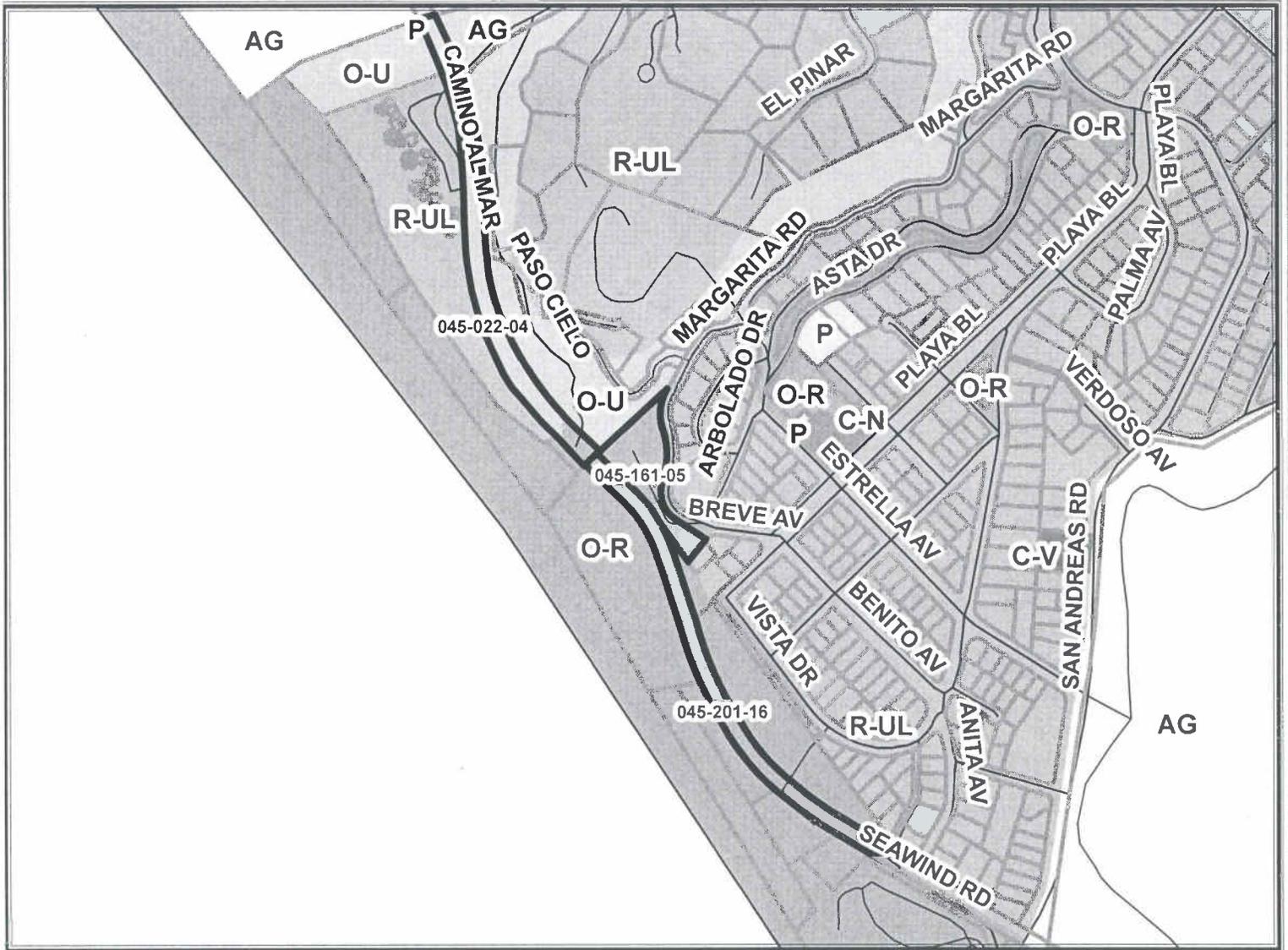
Man Lift & Mini-Excavator
Work Area #2 - 600 Square
Feet Between Bent 6 and
Bent 8

Man Lift & Mini-Excavator
Work Area #1 - 645 Square
Feet Between Bent 3 and
Bent 4

MP 9.09 - La Selva Bridge Man Lift &
Mini-Excavator Work Areas



General Plan Designation Map



LEGEND

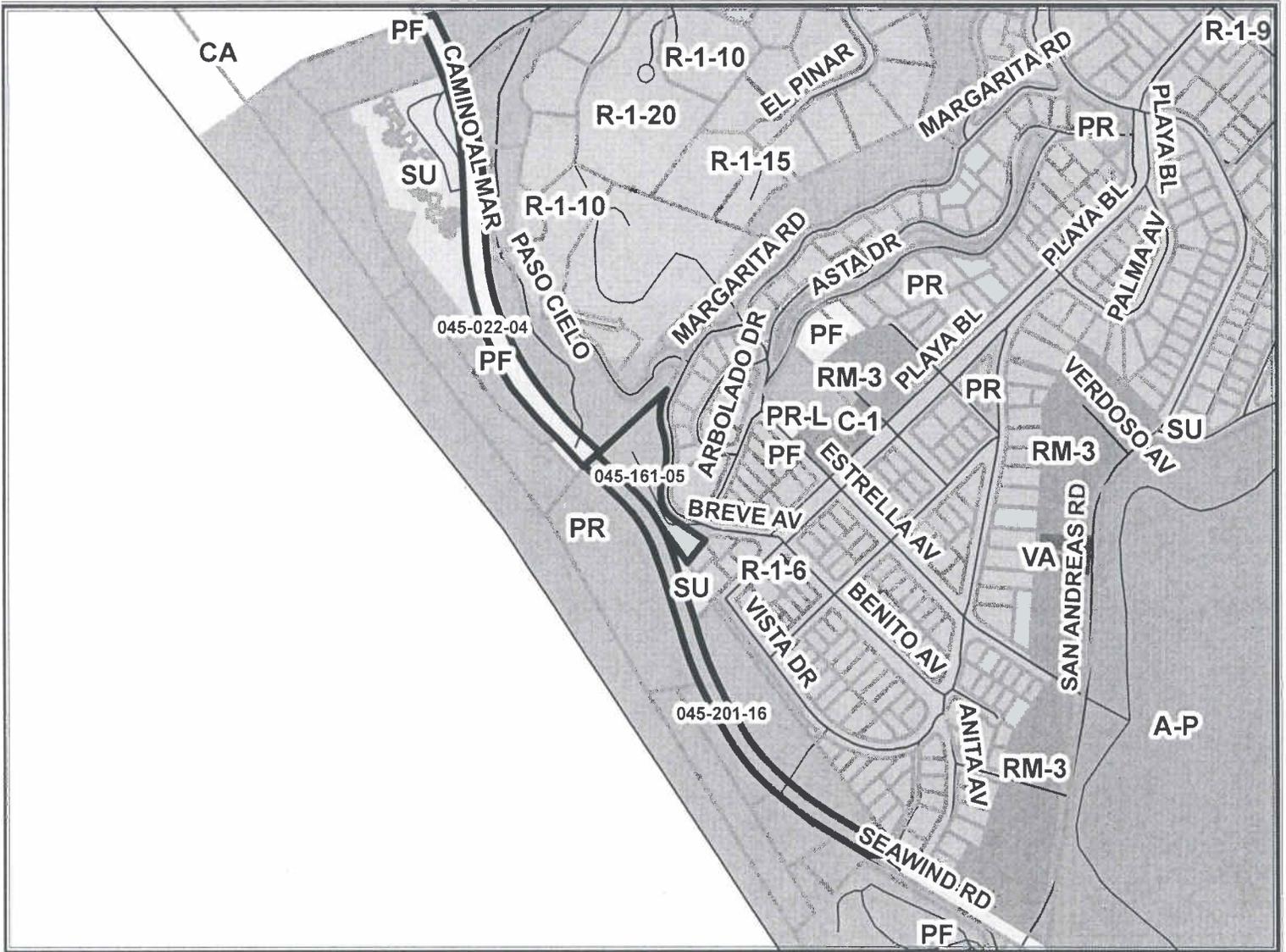
- APN 045-201-16
- APN 045-022-04
- APN 045-161-05
- Assessors Parcels
- Street
- County Boundary
- Agriculture
- Public Facilities
- Urban Open Space
- Residential - Urban Low Density
- Parks and Recreation
- Commercial-Neighborhood
- Commercial-Visitor Accom.



Map Created by
 County of Santa Cruz
 Planning Department
 June 2014



Zoning Map



LEGEND

- APN: 045-201-16
- APN: 045-022-04
- APN: 045-161-05
- Assessors' Parcels
- Street
- County Boundary
- AGRICULTURE COMMERCIAL
- PUBLIC FACILITY
- SPECIAL USE
- RESIDENTIAL-SINGLE FAMILY
- PARK
- RESIDENTIAL-MULTI FAMILY
- COMMERCIAL-NEIGHBORHOOD
- COMMERCIAL VISITOR ACCOM



Map Created by
 County of Santa Cruz
 Planning Department
 June 2014



Location Map



LEGEND

-  APN: 045-201-16
-  APN: 045-022-04
-  APN: 045-161-05
-  Assessors Parcels
-  Street
-  County Boundary



Map Created by
County of Santa Cruz
Planning Department
June 2014



Your plans have been sent to several agencies for review. The comments that were received are printed below. Please read each comment, noting who the reviewer is and which of the three categories (Completeness, Policy Considerations/Compliance, and Permit Conditions/Additional Information) the comment is in.

Completeness: A comment in this section indicates that your application is lacking certain information that is necessary for your plans to be reviewed and your project to proceed.

Policy Considerations/Compliance: Comments in this section indicate that there are conflicts or possible conflicts between your project and the County General Plan, County Code, and/or Design Criteria. We recommend that you address these issues with the project planner and the reviewer before investing in revising your plans in any particular direction.

Permit Conditions/Additional Information: These comments are for your information. No action is required at this time. You may contact the project planner or the reviewer for clarification if needed.

Coastal Commission Review

Routing No: 1 | Review Date: 07/03/2014

NATHAN MACBETH (NMACBETH) : Complete

The comments below were received via email and have been subsequently addressed through further discussions with Coastal staff. These comments have been provided as information only. NMacBeth - Project Planner

California Coastal Commission, Central Coast District
Karen J. Geisler, Coastal Programs Analyst

The proposed project is for the replacement of the La Selva beach railroad trestle bridge and includes grading to create temporary site access and crane pad for construction. The project is located at the intersection of Breve Avenue, Arbolado Drive and Margarita Road and access to the site is through the gate located at the La Selva Beach Improvement Association parking lot.

Please note we have the following comments and questions for you regarding the proposed project as described below.

The project site is located in a parks and recreation area and adjacent to La Selva Beach. Will any components of the proposed project and development including site access and staging areas, be located within a sensitive habitat area such as a riparian or stream corridor? Will the creek be impacted at all? Is there an existing public access to the beach at this location? If so, how will this access be impacted by the proposed project? Will public access to the beach remain open during the project? Or will this access be closed off at any time? Please explain.



Coastal Commission Review

Routing No: 1 | Review Date: 07/03/2014

NATHAN MACBETH (NMACBETH) : Complete

A grading and revegetation plan are proposed as part of the project. Are any trees or existing vegetation proposed for removal in this area? Please identify any trees to be removed clearly on the plans and ensure that any proposed landscaping and revegetation plan includes for only native species to be planted.

According to the notes on the plan sheets provided, C1 states that between October 15 and April 15, the exposed soil shall be protected at all times. Erosion control measures need to be implemented at all times during this project to preserve slope stability and protect water quality. Please ensure these measures are included in the project plan sheets.

Steep slopes are clearly identified within the project site area and the plans identify two proposed locations for the temporary crane pad. The South Crane Site Option #1 appears to be located within the existing La Selva Beach parking lot and the North Crane Site appears to be located within the railroad right-of-way. Will both of these crane sites be used during the project? Please confirm. Will either of these sites impact public access to the beach? Please explain and provide further details such as whether there will be erosion control measures, protections for water quality, best management practices etc. being implemented at both of these crane sites. In addition, with regards to the North Crane option, is all of the work being proposed within the railroad right-of-way?

There are a few pages included with the submitted plan sheets that seem to show the bridge replacement. However, these are difficult to understand with regards to what is being proposed. Will more detailed project plans showing the bridge replacement be provided?

Looking at the La Selva Bridge Erection – Crane Pad plan sheets which include the Shoring Plan and Pile Locations, Elevations and Sections and Detail pages, it seems there is a sheet pile wall but it is unclear what this is. Is this an existing wall? What is the purpose of this wall? Is this shoreline protection? Is it sub-surface and will any portion of it be located on the beach? Does this wall connect with the bridge abutments? Is the bridge replacement for the entire extent of the existing bridge or will just a section be replaced? If so, what are the dimensions of the section to be replaced? Please explain in more detail with a written project description that can be referenced to plans to help us understand all of the proposed project's components.

It is unclear whether there will be any construction work carried out from the beach area. Will there be any construction equipment placed on the beach at any phase of this project? This includes placing any equipment onto the beach sand during construction or bringing the cranes to the project site over the beach. If so, please describe and include details for managing public access to the beach during construction as well as showing any



Coastal Commission Review

Routing No: 1 | Review Date: 07/03/2014

NATHAN MACBETH (NMACBETH) : Complete

construction staging areas or access routes.

Driveway/Encroachment Review

Routing No: 1 | Review Date: 06/11/2014

KEN HART (KHART) : Not Required

Environmental Planning

Routing No: 1 | Review Date: 06/25/2014

KENT EDLER (KEDLER) : Complete

06-25-2014 - Comments by Kent Edler

The application is complete and there are no compliance issues for Environmental Planning Review.

Note to Project Planner, please include the following Conditions of Approval:

Obtain a Grading Permit from the Santa Cruz County Building Official.

Submit a re-vegetation monitoring program to achieve a minimum 80% survival rate of the trees and shrubs after 5 years. If 80% survival rate is not achieved after 5 years, the monitoring program shall continue until 80% is achieved.

An onsite pre-construction meeting must be held with County Environmental Planning staff (831-454-3168), the project civil engineer, the geotechnical engineer, the contractor and the re-vegetation specialist.

A geotechnical engineer must inspect and analyze the temporary cut slope on the southern crane pad prior to use of the pad by the crane. A letter must be provided to the Planning Department by the soils engineer stating that cut slope will not affect the overall stability of the area and is appropriate for the intended use.

All inspections required by the grading permit shall be completed to the satisfaction of the County Senior Civil Engineer.

All site improvements shown on the final approved Grading Permit plans shall be installed.

Final grading for the southern crane pad shall be performed under the observation of a geotechnical engineer. A final letter from the geotechnical engineer shall be submitted to the Planning Department which states that the final grading is stable and non-erosive. Compaction reports shall be submitted to the Planning Department, which indicate a minimum of 90% relative compaction for all fill placement.

Pursuant to Sections 16.40.040 and 16.42.100 of the County Code, if at any time during site



Environmental Planning

Routing No: 1 | Review Date: 06/25/2014

KENT EDLER (KEDLER) : Complete

preparation, excavation, or other ground disturbance associated with this development, any artifact or other evidence of an historic archaeological resource or a Native American cultural site is discovered, the responsible persons shall immediately cease and desist from all further site excavation and notify the Sheriff-Coroner if the discovery contains human remains, or the Planning Director if the discovery contains no human remains. The procedures established in Sections 16.40.040 and 16.42.100, shall be observed.

Yearly re-vegetation monitoring reports shall be submitted annually to the Planning Department for review. An at-cost account with the County shall remain funded for review of the report until the end of the monitoring. Yearly monitoring reports shall be prepared by the re-vegetation specialist.

Project Review

Routing No: 1 | Review Date: 07/03/2014

NATHAN MACBETH (NMACBETH) : Incomplete

1. Please place signs on the properties according to the enclosed *Guidelines for Neighborhood Notification*. Please install 1 sign on APN 045-161-05 at the Las Selva Beach entrance gate clearly visible from the intersection of Margarita Rd and B Ave. Please place a second sign on APN 045-022-04 at the Camino Al Mar railroad crossing. Please use the following text on the sign, take a photograph of the sign installed, and attach it to the installation certificate. The completed certificate returned to your Project Planner. Guidelines for Neighborhood Notification can be found online at: www.scco-planning.com (under the brochures link). If you do not have internet access and require a paper copy, please let us know and one can be prepared for you.

Application #:

141099

Description:

Proposal to replace the La Selva Beach railroad trestle and grade approximately 1000 cubic yards for temporary access and crane pad during construction. Requires Coastal Development Permit and Preliminary Grading Review.

Location:

Property located at the intersection of Breve, Arbolado and Margarita Road through gate (La Selva Beach Improvement Association Parking lot).

APN:

045-161-05

Applicant:

Stacey & Whitbeck



County of Santa Cruz, PLANNING DEPARTMENT

Discretionary Application Comments 141099

APN 045-161-05

Project Review

Routing No: 1 | Review Date: 07/03/2014

NATHAN MACBETH (NMACBETH) : Incomplete

Attn: Michael Dorsa
2800 Harbor Bay Parkway
Alameda, CA 94502
(510) 748-1870
Project Planner:
Nathan MacBeth
Planning Department
701 Ocean Street, 4th Floor
Santa Cruz, CA 95060
(831) 454-3118